

# *Rochester Model Rails*

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A coastal scene from the Mid-Atlantic Narrow Gauge Guild Module Show in Kimberton, PA, May, 2009. Photo by Sam Pennise.

**NMRA LSD Spring Meet – Part II** by Sam Pennise, D.M.D.

**D L & S RR Progress Report – Part III** by Ned Spiller, MMR

**Building a 1:87 Scale Sawmill - Part 14 – Log Carriage** by Richard Senges, C.P.M.

# NMRA LSD 2009 Spring Meet – Part II

## Scott Barody's Gorham & Stanley Railroad

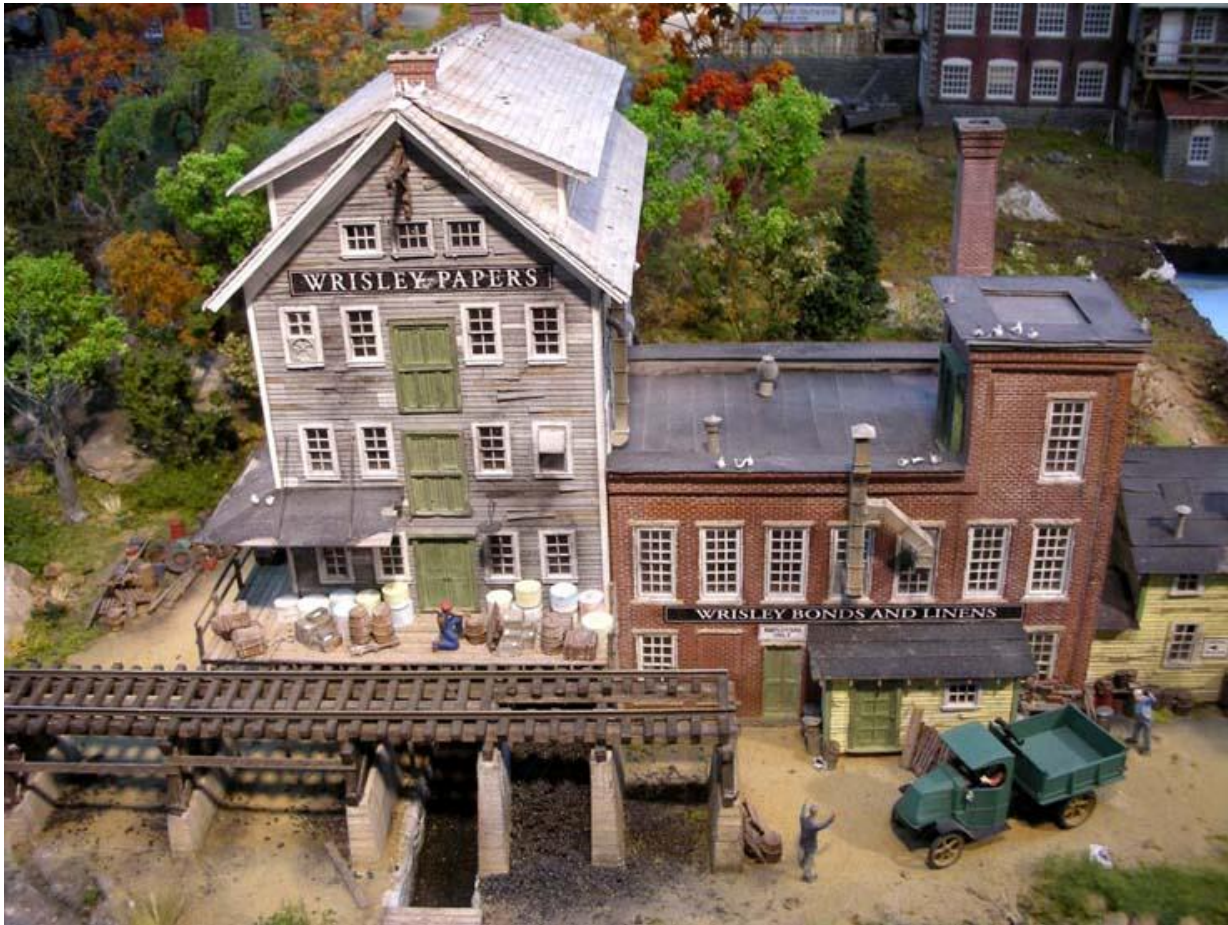
*by Sam Pennise*

The freelanced fictional railroad known as the Gorham & Stanley is set in the Northeast during the steam/early diesel era. As one can see from the pictures, this extensive DC controlled layout features many craftsman kits from a variety of manufactures. Scott has built not only a massive layout of track and rolling stock, but also has built a virtual community of structures that are as professional as this novice has seen to date.

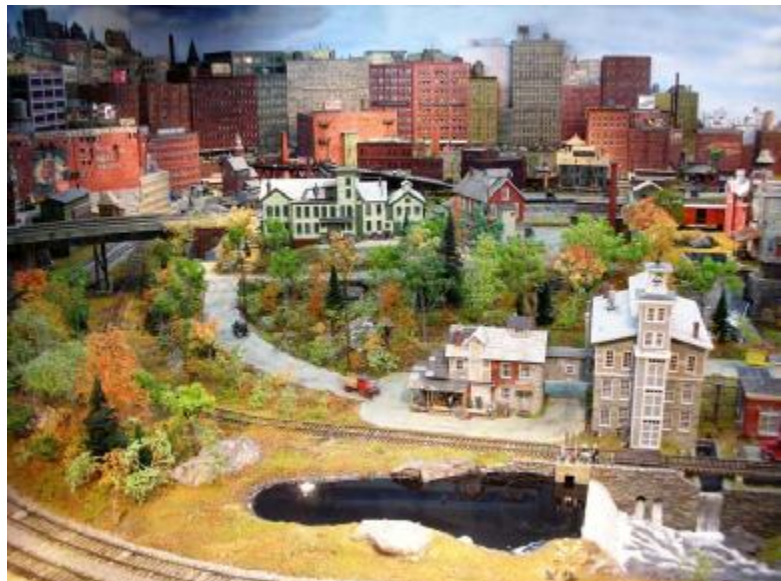
Just look at some of the detail and quality workmanship in some of these pictures. I dare say that with a better photographer one would be hard pressed to tell whether or not what they are looking at is real or a model. The man is not only a master of the rails but truly a professional model builder second to none. (See another image of Scott's layout on the cover of the March/April issue of the *Rochester Model Rails* at [www.OilCreekRailroad.com](http://www.OilCreekRailroad.com) .)



Photo by Sam Pennise.



Photos by Sam Pennise.





The Dolly Varden Mine on the model railroad of Scott Baroody. Photo by Sam Pennise.

As Don and I drove home from Geneva I could not stop thinking how lucky I was to have seen such remarkable layouts and meet such talented people. I guess when someone asks what the NMRA does for me; I just smile and say "a lot".

## Progress Report on the Danby, Ludlow & Springfield – Part III

By Ned Spiller, MMR

For the new section, I used the same methods I used for the original layout. The benchwork is an open grid using 1 x 4s. I cut the sub roadbed from ½ inch plywood, and used the plywood scraps for the risers. I cut several four-foot strips of ½ inch Homosote roadbed on the table saw (outside!). I cut the Homosote 1 ¾ inches wide, then beveled the top ¼ inch at 45 degrees on the table saw. Where the roadbed will curve, I cut a series of 1 inch kerfs about every inch on the inside of the curve. Eventually, I will build the scenery up about ¼ inch on the side of the roadbed, leaving a nice ¼ inch roadbed visible.



Gluing the roadbed to the sub-roadbed.

I am using *Atlas* code 83 track and turnouts for the new section. I've had pretty good success with *Atlas* track in the past, and as long as you prepare the turnouts properly, they give reliable service.

For each of the old sections, I reconnected the track, reinstalled turnouts where necessary, and am patching up the scenery. I am able to run a locomotive over all of the main line from end to end.

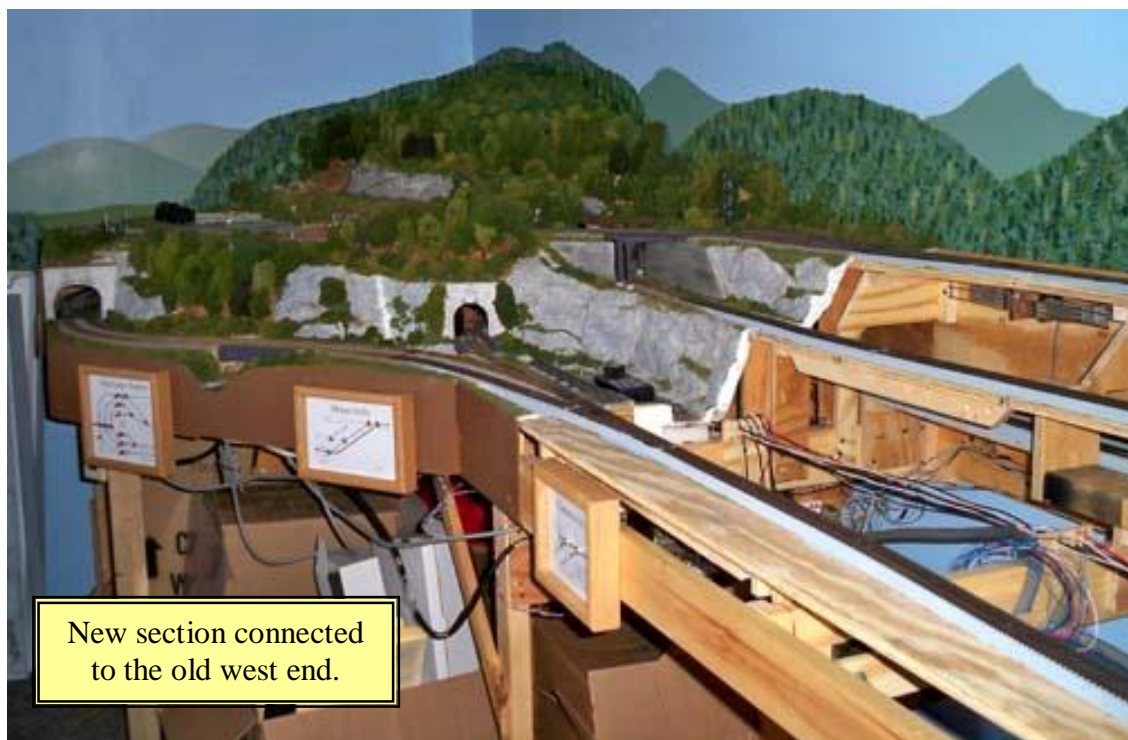
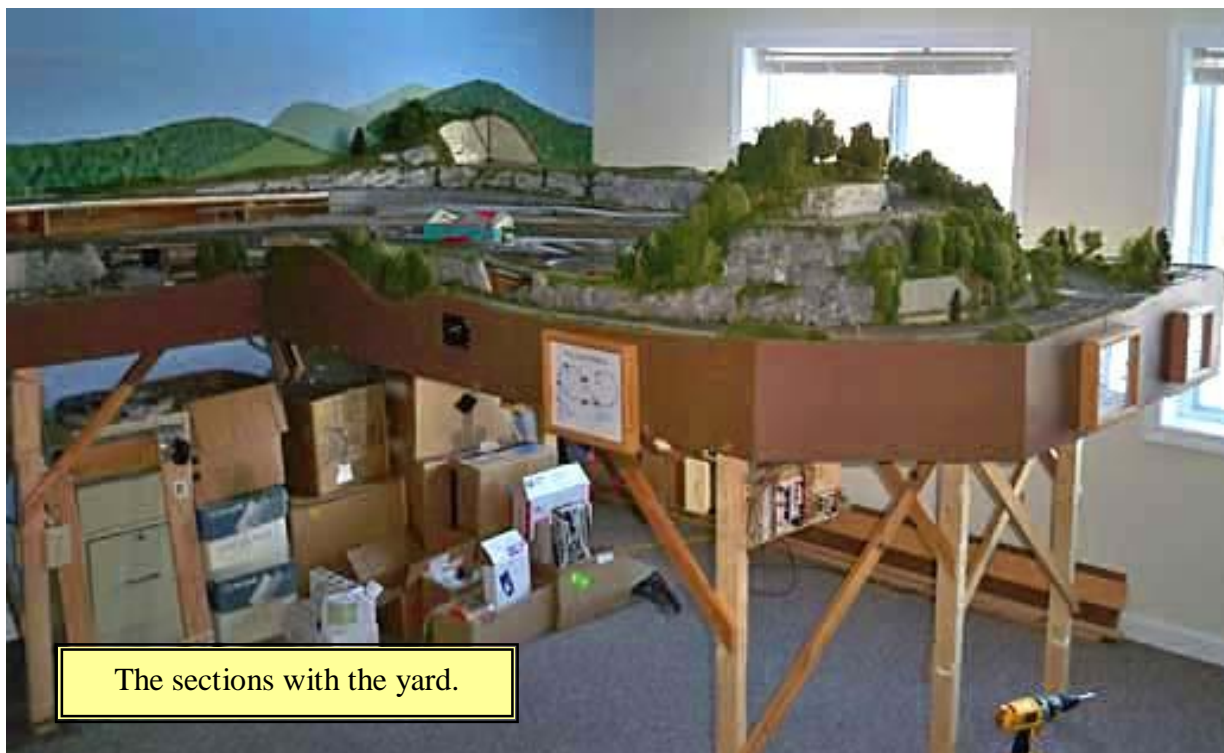
So, that's the progress report on rebuilding and expanding the Danby Ludlow and Springfield RR. I still have to build one more control panel, test all the track, and clean up the scenery. Then I can unpack the rolling stock and structures. Then the big job will be to install sound decoders in the steam locomotives. Finally, I need to redo my car cards and get set up for operations. At that point, I'll be back in service.



This section's joint goes through the roundhouse floor.

Then I get to start on the new section. It will need scenery, some bridges, and a whole town. I do a lot of scratchbuilding, so that could take years. And I still have the future South Rutland industrial switching peninsula to think about.

Lots to do, but I am having fun.



# Builing a Large 1:87 Scale Sawmill I

## Part 14 – The Log Carriage

by *Richard Senges*

In Part 13 of the *Sawmill Series* we discussed the construction of the Trimmer and Slasher. This issue we will review the Log carriage - see the model photo below and the drawing on the next page.

The Log Carriage was primarily built per the instructions in the *SierraWest* kit, but with many modifications. Wheels were especially an issue.

For a prototype look, I followed the drawings in the book "Lumber" by Ralph Clement Bryant. I put the non-flanged wheels on the log side and the flanged wheels on the non-log side. The carriage was built without the platform and provided the operator with controls on the log/saw side (per the actual PA Lumber Museum sawmill). I also added transverse truss rods as per the drawings and pinned the transverse wood support members for added strength.

One less dog was used so that the wheels stood out.

Two carriages were built, one large and one small for the "old mill". On one carriage I used NWSL/64 tread 28" N scale wheel set # 37525-4. Here I pulled the wheels off the axels and mounted the wheels back to back so that both flanges were on the outside, mimicking the prototype type double flanged wheel used on the non-log deck side of the carriage.

### Next Issue – The Lumber Transfer

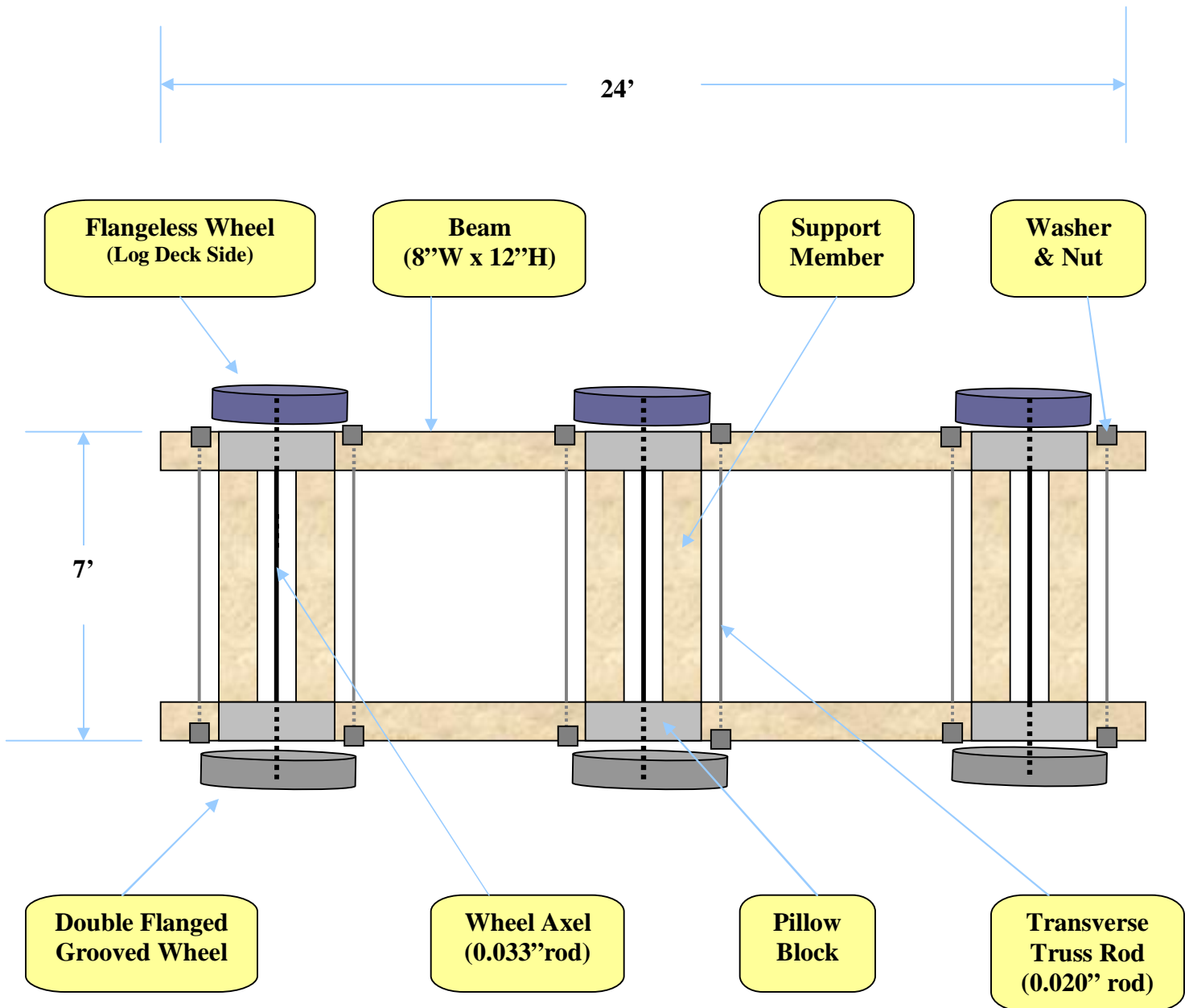


From "Lumber" – "The outer wheel is grooved, and travels on a V-shaped track, with a flat face and sides sloping at an angle of 45 degrees. The wheel rides on the flat face, thereby reducing friction. The wheels near the saw line have flat rims and move on a flat track. The V-rail serves as a guide rail, which ensures the movement of the carriage in a straight line and also keeps the carriage on the track. It is always placed on the farthest side of the carriage, because this form of rail offers greater friction than a flat rail, and the load of the carriage lightest on the farthest side."



# Large Log Carriage Frame

## Bottom View



Note: Actual unit constructed has eight 20" wheels and 4 axels.



Potential Future Articles

*Portable Band Saw*

*Resin Casting*

*The Santa Fe CF - 7*

*Modeling Keuka Lake - Hammondsport*

*Improving Depth of Field*

*Tortoise Installation Made Easy*

*Workbench Construction*

**NEXT ISSUE**

**Mid-Atlantic Narrow Gauge Guild's  
Kimberton Show**

**Building a Large  
Sawmill/Pond Complex  
Part 15 - The Lumber Transfer**

**A Day Out with Thomas at the  
Medina Railroad Museum**

**NMRA NFR Regional Convention**

**Rochester Model Rails**

**E MAGAZINE**

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