

Rochester Model Rails

Dedicated to Quality Model Railroading

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ROCHESTER, NY

APRIL 2004



At the Rio Jct. Engine shops, D & RGW C-16's 268 and 278 are being prepared for the day's activities. Layout of Dave Burroughs. For more great pictures, see his web site at: <http://www3.sympatico.ca/va3cp/>

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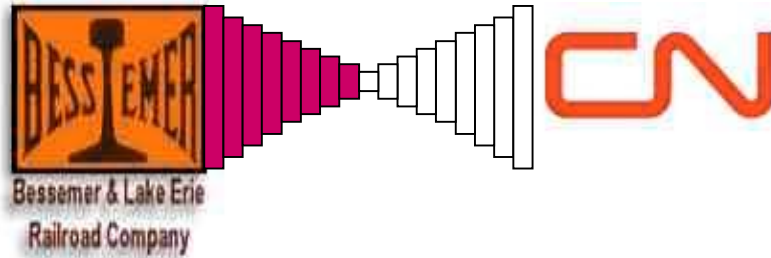
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Notice

All 30 Website addresses listed in the *RMR* are now live. Just click on the blue underlined address and go directly to the Website



Going — Going — Gone

by Richard Roth

That is soon to be the story of the Bessemer & Lake Erie Railroad, a once proud line running north-south in western Pennsylvania and northwestern Ohio. In its heyday, the B&LE sported a number of different steam locomotives pulling passengers and their real money loads of iron ore, coal, coke and limestone from the Lake Erie port of Conneaut, OH, to the outskirts of Pittsburgh, PA. They also interchange with the Erie at Wallace Junction just north of Girard, PA. By June, 2004, the B&LE will have become a part of Canadian National as the giants of the railroad industry today vie for routes that allow them to haul more freight farther on their own rails.

Word has it that the B&LE facilities at Greenville, PA, will also soon be history. The Greenville facility consists of a very large roundhouse along with a number of back shops. They had and continue to have at this time some of the only equipment that can turn steam locomotive wheels or make other repairs. Just south of the Greenville Yard are Shenango Yards. Here, the Bessemer staged their trains for the run north to Conneaut or Girard and south to North Bessemer, PA. Seven or eight tracks, each with compressed air, allowed trains to be made up and ready to pull in short times. Shenango Yard also served as the connecting point with two other railroads, the Erie and the P&LE. Those lines continue to exist today though they are under the control of different operators.

The primary reason for the Bessemer was to haul the raw materials for Andrew Carnegie's steel mills in Pittsburgh. He wanted a secure route from the Great Lakes for raw materials for his mills. In addition to serving as the link between Lake Erie and Pittsburgh, the Bessemer also had a number of branches that allowed loaded coal cars to be brought online from mines in western PA and eastern OH. While some went south to be converted to coke for the mills, a good portion of it went north to be loaded onto ships for destinations around the world. For a more complete history of the B&LE and its predecessors, go to the Great Lakes Transportation website at www.gltx.com. Click on the *History* button on the left then go toward the bottom and click on the *Bessemer & Lake Erie Railroad*.

For the model railroader, the Bessemer represents a very valuable resource that will probably be short-lived once CN takes control. The roundhouse and turntable still exist and are photo accessible. Much of the Bessemer power, all EMD built units can be seen there as well. The fleet was upgraded in the last couple years with SD40's replacing the GP9's, 18's, 30's and even a couple F-units. Their color scheme of black and orange is somewhat unique and very eye-catching.

A short walk, less than a quarter mile, south on the old Erie tracks, now NS (STILL USED SO BE CAREFUL), puts you on a raised vantage point to take photos. (Park at the east end of Canal Street and walk up the slop of the retaining wall there and continue south.) Off to the south a set of bridges can be seen. These form the physical separation between the Greenville Yard and Shenango Yard.

When you are facing the turntable at the roundhouse, there is another set of unique buildings behind you (eastside of the track). Those are the paint shops building for what remains of the Trinity Rail Car Company's Greenville Shops. CSX's "Grain Train" hoppers originated here. Until a couple years ago they turned out covered hoppers and plug-door boxcars in large numbers each year.

Travel south out of town on Rt. 18 and turn left onto Wasser Bridge Road. A couple hundred yards and you cross the right-of-way for the Bessemer and old Erie. Walk north along the right-of-way about a quarter mile and you are at the south end of the Shenango Yard. There you will find Shenango Junction, the confluence of the Bessemer, Erie and P&LE. Look close and you might find the P&LE bills box on a post there. The Shenango Yard is also a favorite spot for the Bessemer to spot strings of their hoppers that carry markings to prevent off-line-interchange. Some of these still have trucks with friction bearings and others that have been converted to roller bearings but show evidence of having had brake shoes on the outside as well as inside the wheels. Most have that part of the side-frames cut off with a torch. A very few still have the ends of the frames intact, but without the outer two brake shoes in place.

Also within walking distance is the diamond where the Bessemer and Erie cross. This area was once very crowded with tracks. Many have been taken up. Some with the ties removed as well. Others have had the rails removed with the ties left in place. This makes it a very good hunting grounds for the date nail collectors as both the Erie and the Bessemer used date nails and continued that practice later than most of the other railroads. The age of many of the ties can be foretold by noting the hot cinder burns in them indicating they have been in place since before steam locomotives gave way to the 1st generation of diesels.

Depending on whom you talk to at the Greenville Yards, you may be told anything from "stay out" to being given a personal tour of the roundhouse and back shops. Crews crawling through Shenango Yard frequently have been known to throw old train orders and other paperwork down to railfans. Now is the time to visit and get those pictures for future modeling projects. Word has it that CN plans to tear down the whole complex and turn it into an Intermodal Facility. That time could be nearer than hope.



Model Railway Association Formed in Canada

By Peter Moffett

Last October, a group of modellers met in St Catharines, Ontario, Canada, and formed the Canadian Association of Railway Modellers. The goals of this Association are:

- To inspire.
- To foster fellowship among model railroaders throughout Canada.
- To provide a forum for the exchange of ideas and skills between model railroaders.
- To promote the hobby of model railroading in Canada.
- To be inclusive of all scales and interests.
- To work in co-operation with other organisations.

CARM has actively started the recruitment of members on a nation-wide basis and has been in contact with railway modellers from across Canada from British Columbia to the Maritimes and in the United States.

The founding principle of the Canadian Association of Railway Modellers is that the strength of this Association lies with the grass roots model railroaders in the Regions of CARM. The role of the National organisation will be to provide support to the Regional bodies in the following key areas.

- Provide national communications tools such as newsletter and website.
- Provide national programs such as promotion and recognition to the Regions for implementation.
- Provide a unified dues structure and maintain a unified membership list.
- Seek out and support an organising committee for a National Convention to be held each year. Encourage the rotation of the convention between Regions.

The Regions of CARM at present are Pacific, Prairies, Ontario, Quebec, Atlantic and International. Membership is open to all model railroaders regardless of their geographic location in or outside Canada.

Membership rates for CARM are:

- General CA\$35* per year and CA\$100 for 3 years
- Youth CA\$20 per year
- Family CA\$40 per year and CA\$115 for 3 years
- Sponsor CA\$150 per year and CA\$400 for 3 years

Membership in CARM is available by using the online application form on the web site www.caorm.org. You can also find a downloadable/printable application form on the CARM web site. Or, you can contact the membership director by email at membership@caorm.org or by writing to:

Canadian Association of Railway Modellers
Membership Director
31-120 Quigley Road
Hamilton
ON L8K 6L4
Canada

Communication with members is critical and a Website www.caorm.org has been created and a newsletter will be distributed bi-monthly commencing with a Jan/Feb 2004 issue.

Modelling skills will be encouraged and promoted and craftsman level skills will be recognized without encouraging competitiveness.

The First National Convention of the Canadian Association of Railway Modellers held jointly with the National Convention of the Canadian Railroad Historical Association will occur in St. Catharines, Ontario, Canada, at Brock University on May 21 – 23, 2004. For details see the Website: www.caorm.org

Founding Board of Directors

John Johnston, Chair	chair@caorm.org
Peter Moffett, Secretary/Treasurer	secretary@caorm.org
David King, Membership Director	membership@caorm.org
Lex A. Parker, Web Master	webmaster@caorm.org

* For USA, \$31.00. For exact exchange rates, contact Peter Moffett above.

[Ed. – Dick Senges of Victor, NY, will be giving two clinics at the National Convention – a scenery clinic entitled “*Fast Track Scenery at the Medina Railroad Museum*” and a tree clinic entitled “*Modeling Conifer and Deciduous Trees*”. For a partial preview of the tree clinic, see *RMR* pages 8 and 9 attached.]

Two Stall Brick Engine House

**Product
Review**

By Dick Senges

In building my new Bath and Hammondsport Railroad, I needed a two stall engine house. I had toured the existing B & H wood frame engine house at Hammondsport, NY, so knew what was needed for the RR. At about the same time I became aware of a 2-stall plastic engine house available from an estate sale. Since I wanted to move forward on the B & H this winter, I purchased the kit designating it for the B & H, at least for now.

The 2-Stall Engine House HO scale kit # 933-3007, manufactured by Pola in Germany, is sold by *Walthers* (www.Walthers.com) as part of their *Cornerstone Series*. The kit consists of: engine house (including 4 wall sections, 2 roof pieces, two end panels & 25 windows) 3 pairs of movable doors, pillar crane, oil drums, driver tires and signs. The footprint is 12 3/4" x 5 1/4".

I ended up with extra doors and windows since I installed doors only in one end of the engine house, not using 5 of the windows and two doors. Optional doors at both ends with one through track is a nice feature of this kit.

The photo (see Page 7) on the box cover shows how the modeler can glue the plastic pieces together and not paint the structure. If constructed this way, all the windows and doors will be bright green, the bricks red, and the roof gray.

Since I desired a different look, the windows, doors, inside of the walls and the underside and edges of the roof were painted with *Krylon* light brown spray paint. Not wanting the roof surface to appear as a metal gray roof, I sanded off the cast lines using disk sander and used black construction paper to simulate a tar paper roof. Later the roof was powered using *Bragdon* (www.bragdonent.com) *Weathering Powders*, giving in a nice weathered look. These two steps resulted in nice light brown windows and doors and a black roof, unlike the photo on the box cover.

Relative the brick sides and ends of the structure, I used *Modeler's Mortar - Concrete Gray* (*Scale Works Models, PO Box 426, Laconia, NH 03247*). This product has a consistency of very thick paint. It is rubbed over the total brick surface and then wiped off leaving the "mortar" in the cracks resulting in a nice mortar effect.

For the windows, I used my favorite product, real glass. Since the windows are relatively large, Ward's (<http://wardsi.com/>) # 14W3245 24mm x 40mm #1 thickness glass microscope slide coverslips were used. Tacky white glue was used to attach the glass to the inside of the window frames.

Black weathering powder was added on the brick just above the engine house doors to simulate smoke soot. All in all, this easy to build 2-stall engine house (see photo Page 7) resulted in a nice model.



The 2-stall Engine House HO Scale kit # 933-3007, manufactured by *Pola* in Germany, is sold by *Walthers* (www.Walthers.com) as part of their *Cornerstone Series*. TM



Model of the 2-stall brick engine house with mortared bricks, powdered tar paper roof, light brown painted window frames and doors, and real glass windows. Track is *Micro Engineering* code 70 flex track with painted and weathered ties.

Photo by Matt Kovacic.



Ask Doctor Dick (The Scenery Doctor)

Ocurr@frontiernet.net

Dan writes:

I want to make good-looking deciduous trees using Peppergrass. How should I do this?

Doc:

Here are the material lists and steps for making Peppergrass trees and also some tips based on experience gained after making many of these trees.

Materials and Tool List:

Peppergrass (sold as Candy Tuft at *Michael's*)
 Floral tape or masking tape
 Wire
 Wire cutter
 Quick grab glue
 Green spray paint
 Acrylic paints – raw umber, raw sienna, burnt umber,
 gray, dark gray, white, black, green
 Stiff oil type paint brush
 Air brush
Floquil paints: Coach Green # 110035
 Burlington Northern Green # 110035
 Reefer Yellow # 110031
 Reefer Orange
 Signal Red

*Doctor Dick's
 Tree/Scenery
 Clinics*

May 1, 2004 -
 Grand Island, NY

May 21–23, 2004 –
 St. Catharines,
 Ontario, Canada

Process Steps:

1. Procure Peppergrass
2. Remove only the large leaves – not the seed pods
3. Separate into smaller sprigs
4. Arrange clumps of sprigs – 3 to 5
5. Wrap floral tape around trunks (masking tape could be used here)
6. Insert wire into bottom of trunk
7. Spray with green paint - this minimizes the amount of paint needed in Step #10. Also, one could eliminate Step #11 below (branch painting) by spraying the branches with a dark color.
8. Apply acrylic paint to trunk (or brown caulk if you want the trunk thicker)
9. Create roots by pulling the acrylic paint (or caulk) out from bottom of trunk
10. Air brush foliage with *Floquil* paint
 - For Summer: 1 part - Coach Green
 1 part - Burlington Northern Green
 1 – 6 parts – Reefer Yellow
 - For Fall: Reefer Yellow, Reefer Orange, and Signal Red
11. Paint branch structure with acrylic paints - brown and gray – various shades
12. Paint trunk with acrylic paints – brown and gray – various shades
13. Let dry and plant tree

“Learnings” From Making Peppergrass Trees

1. When you procure your Peppergrass (sold as Candy Tuff from *Michael's*), try to get bunches that are not crushed, but puffy. Also, buy the natural color and use your 40% - 50% off coupon.
2. When removing the large leaves in Step #2 above, use a sprue cutter, like the one sold by *Micro-Mark* or *The Tool Man*. This makes a nice clean, close cut. Also, make sure you remove all the leaves, because doing it in Step # 11 is a pain! (Do NOT remove the small seedpods at the end of the plant.)
3. Step # 4 is very important because this is where the tree is “formed”. It is important to make sure you like the look of the tree. Make sure the individual sprigs fit well together and the tree looks good to you. Once you tape the tree, you will not be able to rearrange the position of the sprigs.
4. When purchasing floral tape, it doesn't matter if you use green or brown, since you will be painting it completely with acrylic paints (or caulk). Slightly stretch the tape when wrapping. Some folks use masking tape in this step because it sticks more easily when starting the tape. It will be covered in later steps, so masking tape works fine. I have found the masking tape easier to use.
5. When wrapping the floral tape around the sprigs, start with a very small piece of tape. Position the springs exactly like you want them to look. Then complete the taping with a second or third piece of tape. Make sure the trunk taper is designed so the bottom of the tree is bigger than the top.
6. Also, before completing the taping process, this is a good time to insert the wire in the bottom of the tree. I used 18-gauge wire. Somewhat large for a very small tree, but generally works fine. I found 22 gauge too small. Also, I inserted the wire up into the sprig bundle to add additional support to the tree. I also use tacky glue to hold the wire in the tree.
7. When I sprayed with green paint in Step # 7, I used a somewhat darker green than the final green paint in Step # 10 above. Try to aim at the foliage, not the trunk or branches. I also used a special handle for spray cans sold by *New London* (or your local paint store) which made spraying much easier to control. This handle fits most all spray paint cans and is very nice to use. Another trick was not to completely cover all the natural foliage so that a small amount of the natural yellow peppergrass showed through. This gave the effect of lighter and darker colors.
8. When putting the acrylic paint on the trunk in Step # 8 above, cover the tape with the paint so that the tape line does not show. You may have to put the paint on somewhat thick to do this. Also, creating the roots at this time is tricky as the roots tend to get crushed in handling the tree in the subsequent steps. Additional root material can be added in the last step if necessary. Also, taper the tree carefully so you like the final look of the trunk.
9. When mixing the paint for the air brushing, I used ½ bottle of Coach Green and ½ bottle of Burlington Northern Green, and one full bottle of Reefer Yellow. This worked out to 1 part, 1 part and 2 parts. This made a nice light green. After spraying 70 trees, I had most of the paint left in the mixing jar. I used the larger *Floquil* mixing jar and stored the leftover paint in this jar. The airbrush worked fine in that I aimed at the foliage and was able to control where the paint went on the tree.
10. The real pain in the butt step is the last one. This took about 4 or 5 sessions because my rear end got tired just from sitting! Here I painted all the trunk and branch structure with a combination of gray, dark gray, brown, black, green, and white acrylic paints. The hard part here is to paint the fine branch structure without getting the paint on the “leaves”. This can be done, but takes some time. I held the tree in my left hand and painted with a small long-handled brush. Sometimes I stuck the tree on a foam pad and separated the foliage with my left hand, and painted with my right hand. I tried to vary the trunk color on each tree. On some trees, I sprinkled some ground up real bark mixed with dry gray poster paint powder to add texture and color to the trunk. (Note: an easier alternative to this step is to spray just the branches with dark color paint, i.e., insert *Process Step 7a*.)

WANTED

Model Railroaders

Interested in Model Railroad Scenery

The Group: *A small group of model railroaders is being formed in the Rochester, NY area.*

The Goals: *Have fun.*
Improve scenery skills.

The Interest: *If interested, contact:*
Dick Senges – OCRR@frontiernet.net
or
Matt Kovacic – mkovacic@rochester.rr.com

GUIDELINES FOR GOOD PHOTOGRAPHIC COMPOSITION

or

HOW TO MAKE GOOD PHOTOS BETTER

By Leaf Shutter

Guideline No. 6 **Subjects in Motion**

A subject in motion should have space in front of itself in which to run. Never position the locomotive pilot at the absolute edge of the photograph. Allow some space for it to run into. It is sometimes good to leave greater space in front of the moving object than behind. This may be impractical with long trains.

For back issues of the

Rochester Model Rails

See the website

www.trainweb.org/rmr

Coming Next Month

Virginia is for Layouts

Rail Museums

Product Review – Barns

**How to Make Good Photos Better
– Guideline #7**

Ask Doctor Dick
(the Scenery Doctor)

Rochester Model Rails

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Mailing Address
1231 Wellington Drive
Victor, NY 14564

Web Site: www.trainweb.org/rmr

Recommended Train Events for 2004 – Updated 2-26-04

March 7	Niagara Falls, Canada – Niagara Falls Model Railway Show
March 20-21	Rochester, NY - Rochester Model Railroad Club Open House
March 21	Rochester, NY – RIT Spring Train Show
April 4	Batavia, NY – Train Show/Sale at Batavia Downs
April 24	Sonora, CA – 17 th Annual Logging Modeler's Convention
May 1	Grand Island, NY – International Division Meet – <i>Doctor Dick's Tree Clinic</i>
May 21 – 23	St. Catharines, Ontario, Canada – National Convention – Canadian Association of Railway Modellers. For details: www.caorm.org <i>Doctor Dick's Tree Clinic and Scenery Clinic</i>
June 11-13	Tacoma, WA – Northwest Logging Modeler's Convention
July 3	Medina, NY – American Military Weekend, Medina Railroad Museum
July 3 - 4	Galeton, PA – PA Lumber Museum Bark Peeler's Convention
August 4-8	Chantilly, VA - N Scale Collector's Convention 4 th – 7 th - N Scale East Convention 5 th – 8 th - www.nscalecollrctor.com
August 19-22	Durango, CO – Railfest 2004 – Durango & Silverton Narrow Gauge RR
August 21-22	Rochester, NY - Diesel Days at the Museums: NY Museum of Transportation and Rochester & Genesee Valley Railroad Museum
September 1-4	Santa Clara, CA - 24 th National Narrow Gauge Convention
November 6 – 7	Syracuse, NY – Train Show at Fairgrounds
November 19	Rochester, NY – NRHS Meeting – “ <i>History of the Oil Creek Rail Road</i> ”
November 14	Batavia, NY – Train Show/Sale at Batavia Downs

For a detailed listing of events, go on the Internet to:

WWW.CAORM.ORG

Shows
Look for dates and location

WWW.RAILROAD.NET

Events
Look for date and location

WWW.GATS.COM

Great American Train Show
Show Schedule
Month of Year
Look for your city

WWW.TTOS.ORG

Calendar
Month of the year
Look for your area

WWW.GSMTS.COM

Great American Model Train Show
Dates and Events

WWW.TRAINS.COM

Schedule of Events
Events

WWW.MODELRAILNEWS.COM

Events
Look for your area

WWW.FINGERLAKESLIVESTEAMERS.ORG

Events